



# OYSTER BAY

Design Guidelines

2015









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# 1

## Introduction



## Introduction

### Purpose of design guidelines

The design guidelines set out in this document are intended to guide, shape and inform the layout and design of proposals that come forward as part of the development of Oyster Bay (IR 12). The document contains six sections. In addition to the introductory section, the guidelines provide design principles and specific guidance for a range of land uses and development types relevant to Oyster Bay.

### Design principles for Oyster Bay

Oyster Bay is an attractive site characterized by mature forest and frontage onto Ladysmith Harbour. There are significant opportunities for new development to capitalize on the natural assets of the site (and its highly accessible location), to ensure a range of uses are brought forward within an attractive setting.

The overall design principles for Oyster Bay are to:

- Capitalize on the natural setting of the site including the mature forest and the coastline;
- Create a visually attractive development with a distinct sense of place;
- Create a mix of land uses, including residential, office, commercial and community uses;
- Provide a range of housing opportunities and choices; and
- Promote a compact, walkable urban form.



1. Existing trees should be retained where possible
2. Existing natural features should act as form-givers to new development

## Placemaking

Placemaking is the process of bringing individuality and distinctiveness to the built environment in order to produce a sense of place. Responding to local character and context provides a starting point for creating a unique sense of place. Development proposals should be informed by a site appraisal, which will uncover in detail the opportunities and constraints that should be taken into account when developing any particular part of Oyster Bay. There are many existing features, such as views across Ladysmith Harbour, the interesting coastline and existing vegetation, which should act as form-givers for the development with the purpose of assisting in achieving uniqueness of character.



1. Views across Ladysmith Harbour should be capitalized upon
2. Development proposals should be informed by an appraisal of the site









# 2

## General Design Guidelines



## General Design Guidelines

### Overall layout

Development proposals should be based around an inter-connected network of streets, footpath / cycleways and open spaces in order to promote a high level of connectivity and permeability. This will make it easy for people to find their way around and will ensure residents and visitors are within easy reach of local amenities and facilities. Proposals should make full provision for movement by private car, on foot and by bicycle.

Oyster Bay should be developed as a series of distinctive, yet interconnected neighbourhoods. Each neighbourhood should have a clear centre, or focal point (usually in the form of a park, a school or commercial area), to promote identity and community organization. The village centre will form the overall 'heart' to the development providing a variety of residential and commercial uses and community facilities.

Neighbourhoods should be designed for ease of walking. A walkable neighbourhood is usually defined by the time it takes people to walk to local facilities and amenities. There should be neighbourhood parks within a five minute walk (400 metres) and local shops, health centre and perhaps a school within a walking distance of, say, 10 minutes (800 metres).



1. Neighbourhoods should have a clear centre or focal point
2. The layout and design of neighbourhoods should promote walking and cycling as well as movement by car



### Edge interfaces

Each neighbourhood should have strong physical and visual links to natural environmental features including, for example, the edge of Ladysmith Harbour, areas of retained trees and Bush Creek. Interfaces between neighbourhoods and natural amenities, open spaces and other features should be positive to provide views, access and connections. Along Ladysmith Harbour, for example, buildings should ideally front the water, rather than back onto it, and public access in the form of a pathway or trail should be provided.

Along the coastline, maintaining native plantings will be essential to:

- maintain the natural character of the site by retaining existing plant material that is consistent with surrounding areas;
- enhance interpretative opportunities and support the interpretative program that will be established;
- limit erosion and sedimentation concerns along riparian edges; and
- provide a wildlife corridor along this interface which will be of the most species rich and abundant ecosystems in the final development.

Along the Trans Canada Highway, pockets of native vegetation will be retained in order to maintain the natural character of this corridor. Limited clearing of native vegetation along this edge will be considered for improved sightlines to entrance signage and access points, and other major sightlines required into the site such as the north-south Main Street view from the Trans Canada Highway.



1. Housing fronting onto parks and open spaces creates an attractive edge and provides overlooking
2. New housing fronting onto an area of retained trees can create a positive interface



### Street hierarchy and road design

Within the overall design for Oyster Bay there should be a clear hierarchy of routes and public spaces. Building scale, landscaping and the use of materials should be used to provide an understanding of this structured framework.

Streets should be of an appropriate width to reflect their use and importance and should provide a positive outdoor space (be a place for people not just a channel for motorized traffic). It is important that all streets are clearly defined and enclosed by built form or landscaping (typically in the form of retained trees).

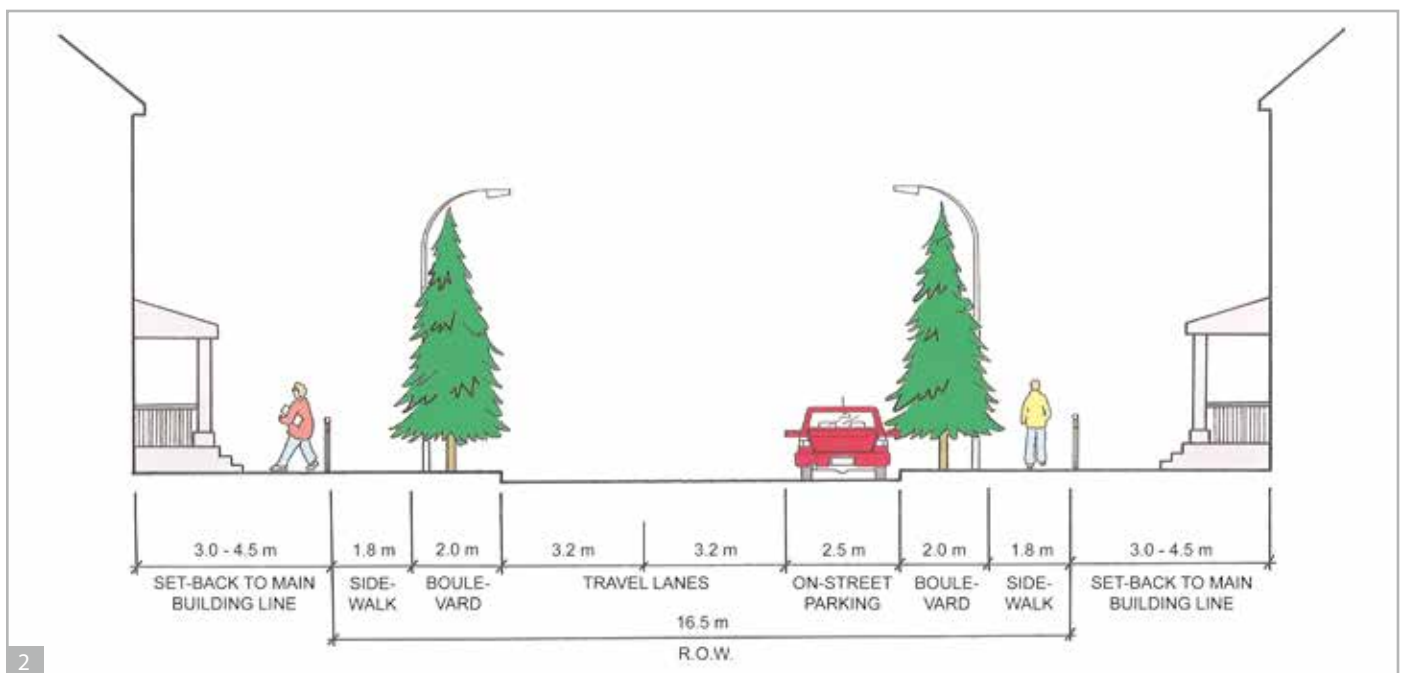
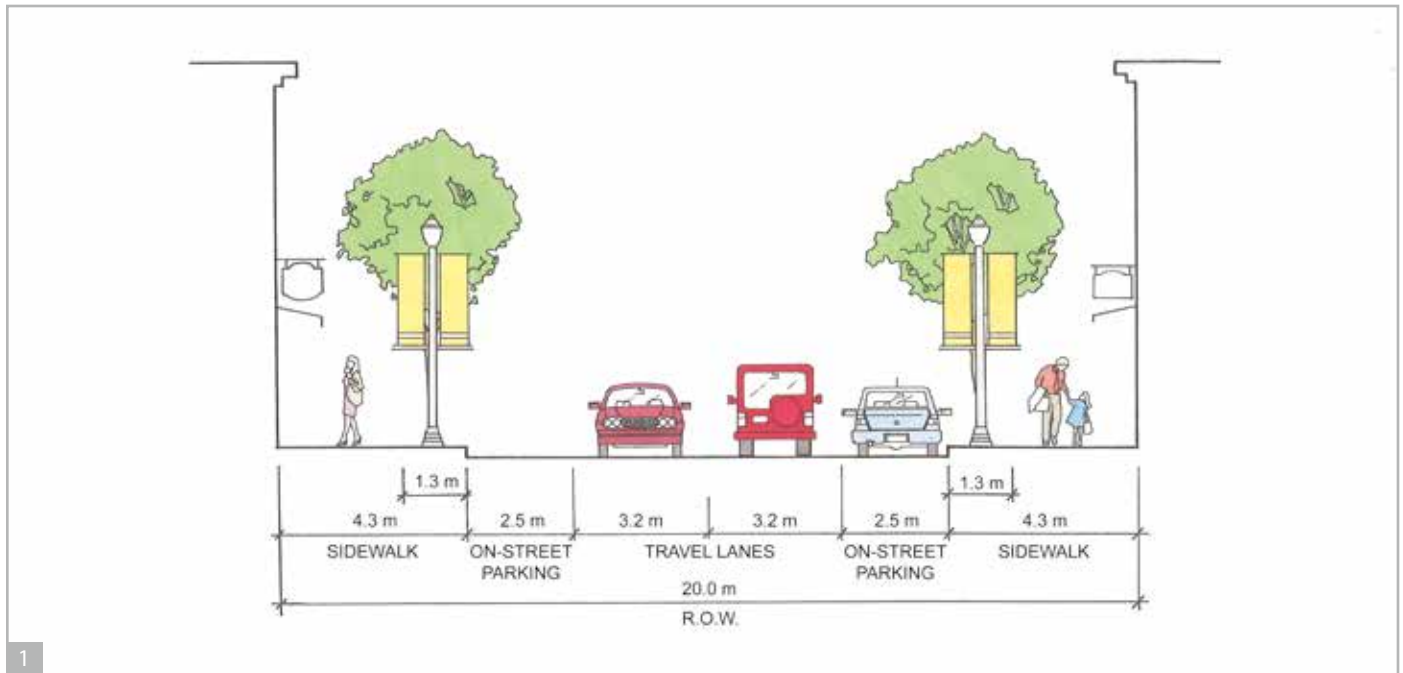
The philosophy should be to recognize but control the car. The aim should be to prioritize the pedestrian and cyclist by constraining traffic speeds and accommodating parking provision in a sensitive manner. These objectives should be achieved through the layout and design of streets rather than more commonly used modern traffic calming and control measures, which can create a cluttered streetscape.

To provide a framework for the development of consistent road standards across Oyster Bay, four cross sections have been established to provide direction on matters such as typical right-of-way widths, pavement widths, boulevard widths, sidewalk widths and landscaping.

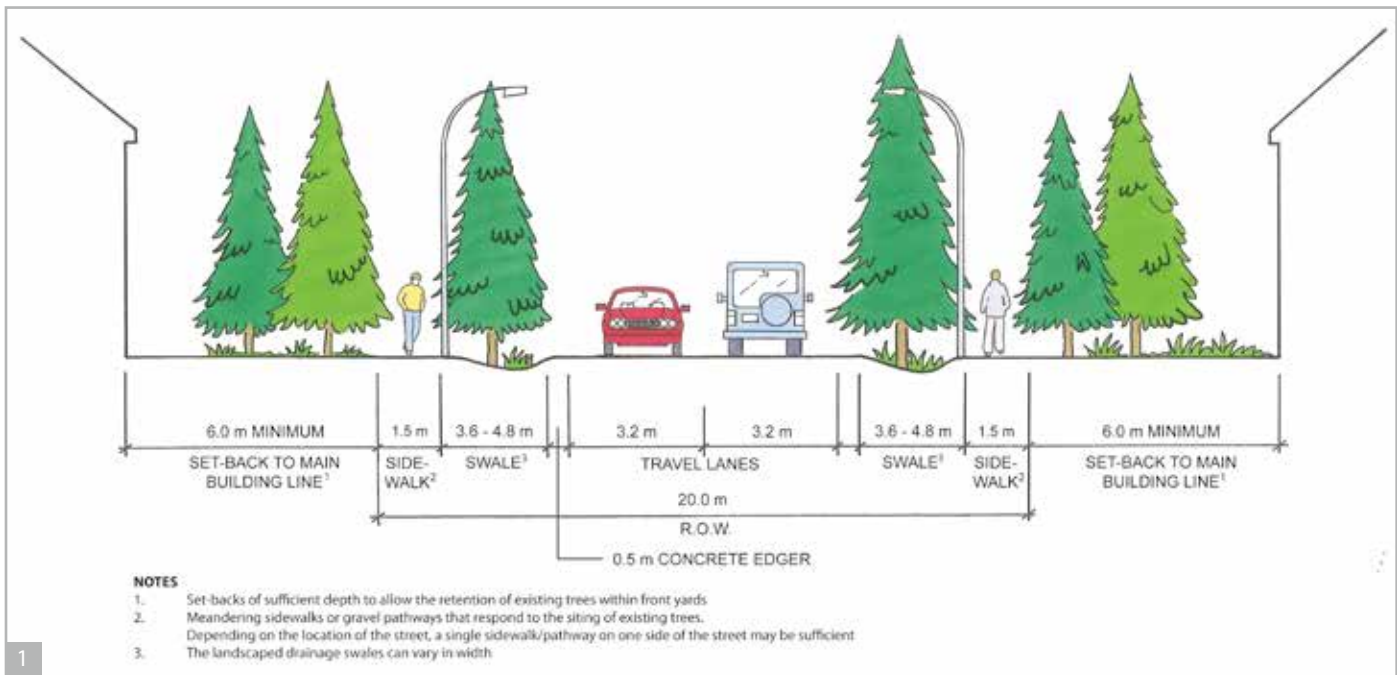


1. Within the core of the village centre there are opportunities for buildings to define and enclose streets
2. Outside the village centre core there are opportunities for streets to be defined by landscaping (this will typically take the form of retained trees)

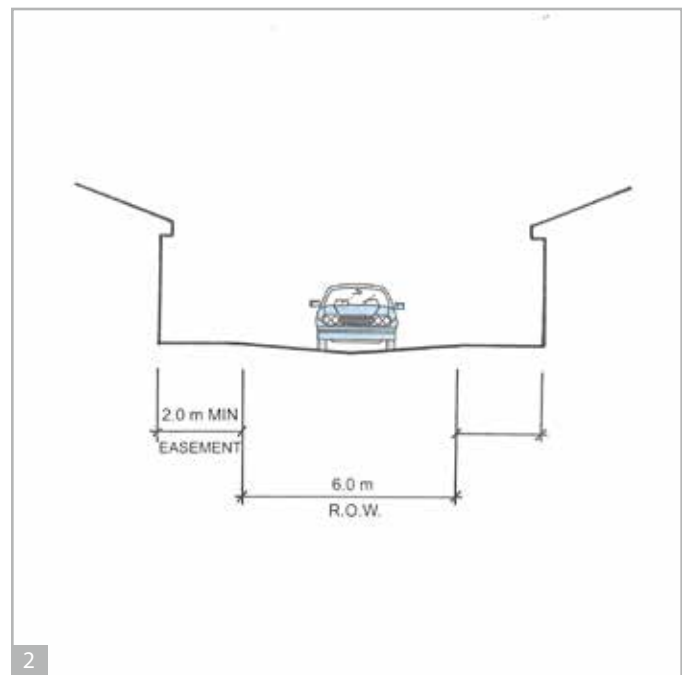




1. Illustrative street cross-section for Main Street
2. Illustrative street cross-section for local residential streets (urban)



1



2

1. Illustrative street cross-section for local residential streets (rural)
2. Illustrative cross-section for residential laneways



### Block design

Blocks define and structure neighbourhoods. They influence movement patterns, development opportunities and neighbourhood character. The size and shape of blocks should respond to natural features, capitalize on views and contribute to the creation of a distinct sense of place.

Connectivity and permeability should be key considerations when determining the length of individual blocks. Long blocks can act as barriers to pedestrian movement and, therefore, should generally not exceed 250 metres in length.

Mid-block connections are one way of encouraging pedestrian permeability on longer blocks. The connection should be at least 3.5 metres wide and include pedestrian scaled lighting. Side elevations on either side of the connections should contain windows to ensure a degree of passive surveillance.

Blocks incorporating laneways can be used to eliminate the need for driveways and street facing garages.



1. Mid-block connections can improve pedestrian permeability. They should be a minimum of 3.5 metres wide and be overlooked by surrounding dwellings
2. Rear laneways negate the need for front garages and driveways and can help create attractive streetscapes

#### Lot size and variety

Lot size and shape have a direct impact on the type of houses that are possible, which in turn influences site density, buildings costs and affordability. The aim should be to provide a range of lot sizes in order to promote a variety of house types (in terms of size, design, character and affordability).

Lots should generally be simple and rectilinear to reduce constraints on design and siting options. However, irregular shaped lots, such as pie-shaped lots, and variations to the traditional lot, such as wide shallow lots, will be appropriate in some locations.

#### Architectural style

These design guidelines are not intended to prescribe a particular architectural style. However, the aim is to ensure that new development sensitively integrates within the natural setting of Oyster Bay. Building styles that respond to the landscape features of the site are, therefore, likely to be most successful. Strong connections to the natural environment with respect to the form and scale of buildings, the use of natural materials (eg. timber and stone), and the siting of buildings (eg. to maximise views of natural features) is encouraged. The aim should be to use traditional built forms and materials, but in a contemporary manner to produce fresh, modern designs.



1. Buildings should be sensitively integrated into the natural setting of Oyster Bay with trees retained where possible
2. The use of natural materials is encouraged



### Built form and massing

Building elevations represent the main interface element between public and private. Maximizing windows and doors encourages active frontages, enables communication between inside and out and facilitates natural surveillance and overlooking. Devices such as balconies, canopies and bay windows can be incorporated onto elevations to help buildings reach out into the public realm.

Corner buildings are visually prominent, have two frontages and have the potential to form strong townscape elements. They, therefore, require careful detailed design. Tailored designs should be used to ensure corner buildings address both street frontages.

### Parking

Parking provision for motor vehicles should respect the built form of the development and be incorporated in an unobtrusive manner. The number of parking spaces per dwelling will inevitably vary depending on the type of accommodation being provided. A range of alternative parking arrangements should be considered. However, favoured arrangements will be parking facilities that do not have a detrimental effect on the relationship between buildings and the street.



1. Windows on side elevations provide overlooking of residential side streets
2. Front porches and balconies help to animate the public realm

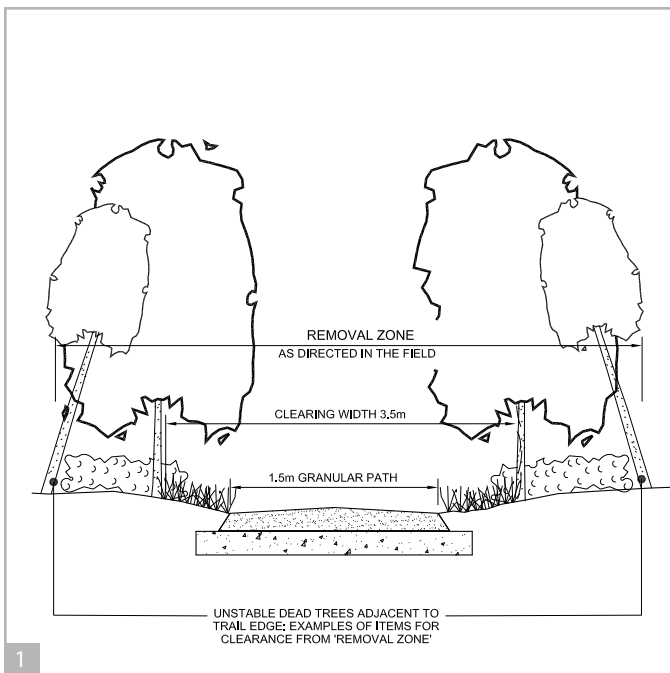
### Design of pathways and trails

A variation of trail types will be implemented throughout the study area that are suited to the intended use, adjacent built form and natural character. Overall, more formal trails will accommodate intensive and multi-modal use, while passive trails will be built through natural areas and along the waterfront.

Along the water's edge, a wood chip or aggregate surfaced trail approximately 1.5 metres in width will provide access for passive trail use and exploration of the natural areas of this site. Along this trail, viewpoint promontories, such as short boardwalks or viewing decks, will be built to allow for strategic views of Ladysmith Harbour and controlled access to the water's edge. The primarily wooden structures will serve as focal points of the pathway and trail system, and will be destinations for locals and tourists out cycling, walking and jogging in the community. A width of approximately 1.0 metre on either side of the trail should be cleared of all shrubs and tree branches to allow for the safe passage of trail users. This clearing area shall be vegetated



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1. Illustrative cross-section of a trail with a 1.5 metre path
2. A variety of trail types will be constructed
3. Viewpoint promontories will be built to allow for strategic views of Ladysmith Harbour and controlled access to the water's edge



with native grasses and other groundcovers. Benches and waste receptacles should be located no more than 200 metres apart, facing the more prominent view available. Limited lighting will be provided at any waterfront viewpoint structure, but not along the trail itself.

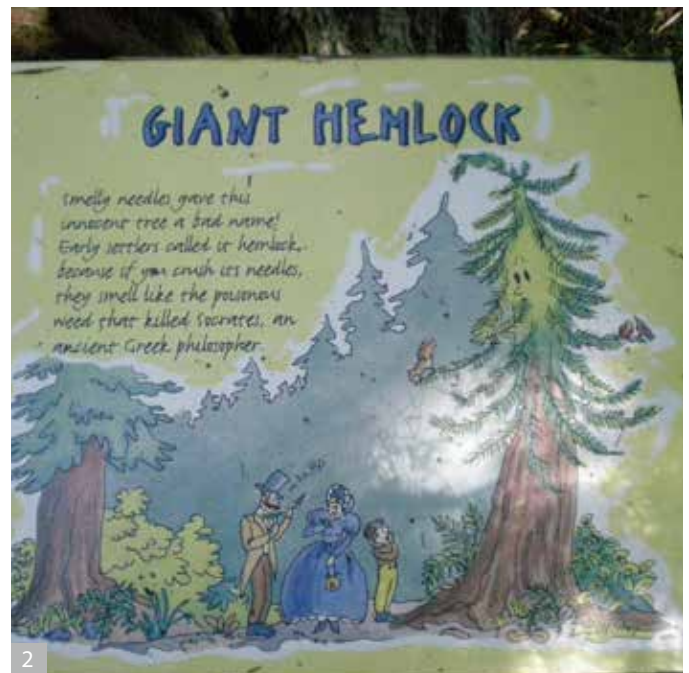
A multi-use trail of minimum 3.5 metres of paved surface will lead through the development, across the new Bush Creek bridge and toward the southeast land area. This trail connects to the regional trail system and is excellent for walking, jogging, cycling and enjoying the west coast wilderness. A minimum 1.0 metre wide buffer on either side of the trail shall be cleared of all shrubs and tree branches, with native grasses and other groundcovers within this cleared zone. Rest nodes, consisting of two benches and a waste receptacle, should be spaced no more than 250 metres apart along this trail.

Pathways leading through the development area, not including sidewalks along streets, shall be a minimum 2.0 metres width paved surface to allow for intensive use and ease of wheeled modes such as cycling, roller blading, and pushing a stroller.

### Interpretation and signage

Residents of and visitors to Oyster Bay will be in close contact with sensitive and special eco-systems. Encouraging stewardship of the environment through a local education program will help protect the local flora and fauna and reduce future maintenance or reclamation requirements. Interpretive signage throughout the trail system and in the tourist zone could educate tourists and residents about the local habitat, eco-systems and the animals that depend on it. Interpretive signage should be located along the waterfront trail interface, highlighting the various themes related to local ecology such as:

- tides, and their effect on riparian ecosystems;
- native forest stands and forest succession;
- fish habitat and spawning processes along shorelines; and
- wildlife of adjacent upland areas.



1. Maps and signage should be provided to help visitors navigate their way around
2. Interpretive signage should be provided as part of a wider strategy to educate residents and tourists about the local environment









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## Residential & Assisted Living

## Residential -Single Family Housing & Assisted Living - Patio Homes

### Orientation

All single family houses should front adjacent streets and open spaces to provide a positive residential image and natural surveillance of the public realm. Elements that have a negative impact on the overall character of the streetscape – such as front-attached garages that dominate the front of a house and blank elevations – must be avoided.

Corner lots have two frontages and are, therefore, visually prominent. Single family dwellings on corner lots should be designed so that they address both frontages and suitably 'turn the corner'. This will mean two attractive elevations, containing windows to provide overlooking.

### Building elements

- Windows – the aim should be to achieve a well-composed façade with a good balance of windows to solid wall. Large 'picture windows' with unsubdivided panes of glass should not be used as they distort the visual scale of the building.
- Doors – front doors should be orientated to adjacent street. This ensures that entrances are clearly visible and identifiable.
- Roofs – should generally be pitched and kept simple in style. The over-use of feature gables, which creates a 'busy' roofscape, should be avoided. The size and placing of dormers should be carefully considered. Bulky dormer structures can make them over-dominant in the overall building composition.
- Chimneys – traditionally, chimneys played an important architectural role in the composition of houses and should, therefore, be of a suitable scale. This will enable them to contribute to the overall structure of the building. They should be constructed of stone or brick to create strong vertical elements. Exposed metal flues will only be permitted facing a rear yard.
- The over-use of 'skin-deep' details such as applied gables, false chimneys and bay windows of minimal projection undermines the integrity of buildings and should be avoided.



1. Houses should front adjacent streets and open spaces to create a positive edge and overlooking
2. The use of natural materials is encouraged on single family houses



- Materials – the use of natural facing materials such as timber and stone is encouraged. Brick and stucco is also considered acceptable. The aim should be to use a limited palette of materials on any individual building. Changes in facing materials on a house should occur in a logical fashion: from one storey to another, to articulate part of the structure (eg. a front façade), or an architectural feature (eg. a feature gable or a bay window). Constructional elements such as lintels can also be enhanced by picking them out in a different material. Changes in materials and detailing should be used in such a way as to help explain the structure of the building.

### Driveways and garages

It is common practice for single family dwellings to have front attached garages. However, this arrangement can have negative impacts when the garage dominates the front of the house. It can lead to streetscapes where garage doors and driveways become the main visual element. It also limits opportunities for positive design features such as front porches and front facing rooms, and reduces natural surveillance of the street from the house.

In order to reduce the impact of front attached garages on the façade of the house and on the wider streetscape, they must be no wider than 50 percent of the width of the house. Also, there should be no significant projection of the garage beyond the primary façade of the house (maximum 3 metres). Wide shallow lots allow the provision of front attached garages and front facing rooms.

Alternatives to front attached garages are detached garages located at the side or rear of the house. Rear detached garages can be located off a lane or positioned immediately behind the dwelling and accessed from a front driveway running along the side of the house.

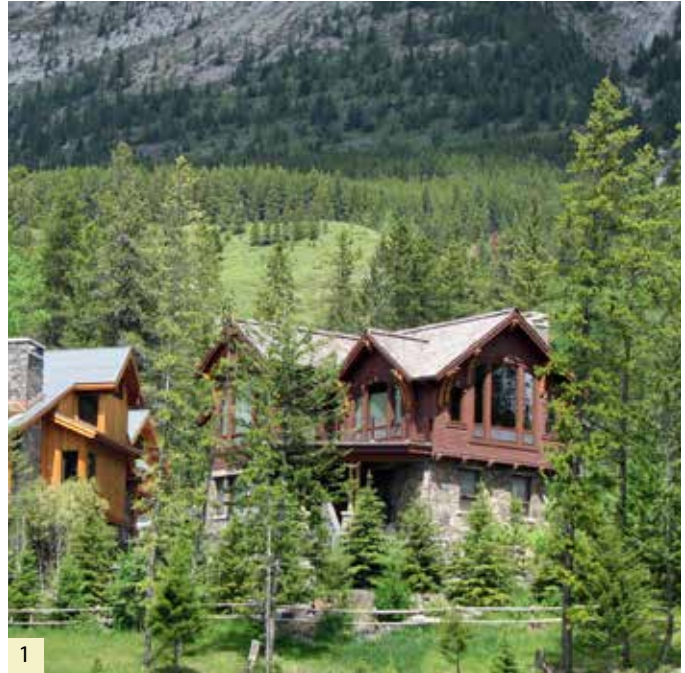
A range of parking solutions is encouraged to add variety and interest to the development.



1. Wide shallow lots allow front attached garages and street facing rooms
2. Rear detached garages should be attractively designed using similar materials to the main house

## Landscaping

Where possible, residential lots shall retain native plants including trees, shrubs and groundcovers, where building footprints, driveways and site grading does not impact these areas. Maximizing native plantings will help tie these developed parcels into the surrounding natural landscape and maintain the character of the overall development. Landscaping of disturbed areas shall be predominantly native plant material, with mowed turf being acceptable within private lot areas. Any disturbed areas outside the lots caused by site grading shall be landscaped with only native plant materials and groundcovers.



1. Residential lots should retain native plants to allow buildings to integrate with the natural setting of Oyster Bay



## Residential - Multi Family Housing & Assisted Living & Extended Care Units

### Orientation

There are opportunities within Oyster Bay to accommodate a number of apartment buildings. Some of these building, depending on their location, could accommodate retail or office space at the ground floor. In all cases apartment and mixed-use buildings should be human in scale and relate well to their surroundings.

Apartment buildings close to the village centre should play a role in defining and enclosing streets and other public spaces. In some areas located further away from the core it may be justifiable to set buildings back, especially where there is a desire to create landscape, rather than building, dominated streets. Regardless of set-back distances, buildings should be orientated to the street with a clear set of entrances.

### Building elements

- Elevations – apartment building should be designed with a clear base, middle and top. The use of materials and the design of individual architectural elements can be used to emphasize the transition from the base of a building to the top. Each elevation should have visual interest from a range of viewing distances. The articulation of elevations should have relief, depth and shadow. It is important to emphasize vertical rhythm in particular and avoid blank walls and elevations that appear flat.
- Doors – front doors should be orientated to adjacent streets to ensure that they are clearly visible and identifiable. Ground floor residential units should have individual at grade entrances onto the street where possible. This helps to emphasise the residential nature of the building. In mixed-use buildings, residential apartments should have a separate, clearly identifiable entrance.
- As with the design of single family houses, the over-use of ‘skin-deep’ details on apartment buildings, such as applied gables and bay windows of minimal projection, should be avoided.



1. Some apartment buildings could accommodate retail or office uses at ground floor
2. Ground floor residential units should have individual at grade entrances

- Materials – the use of natural facing materials such as timber and stone is encouraged. Brick and stucco is also considered acceptable. Apartment buildings should be finished in a simple high quality palette of materials, which when used in conjunction with each other should aim to break down the overall height and mass of the elevations.

#### Shared amenity spaces

Apartment buildings should have some shared private outdoor amenity space associated with them for the enjoyment of residents. This could be provided as an external garden area (to the front, side or rear of the building), or in the form of a rooftop terrace / garden. The space must be useable: it should have a degree of privacy, be safe, receive direct sunlight and be sheltered from the wind.



1. The use of natural facing materials is encouraged
2. Parking for multi-family units could be accommodated to the rear accessed off laneways



**Parking**

Apartment / mixed-use buildings will be served by surface parking. The visual impact of these parking areas should be minimized by limiting the number of stalls in a single continuous run and allowing adequate space for tree planting, which can be the most effective way of integrating parking into the urban landscape.

**Landscaping**

The landscape design principles outlined in relation to single family lots also apply to multi-family parcels.



1. Townhouses could incorporate parking at ground floor level
2. Multi-family lots should retain native trees where possible









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Commercial

## Commercial Development

Commercial Developments often combine retail at ground floor with residential or office above. This could be in the form of two-three storey buildings along an urban street (Main Street) at the heart of the village centre or along the Trans-Canada Highway.

Commercial units located off the Trans-Canada Highway should be easily accessible to passing trade. However, this does not mean that they should be solely orientated around the car. Units should be accessible to pedestrians from the village centre and adjacent neighbourhoods. Buildings should be organized to define and enclose adjacent streets, internal drive aisles, sidewalks, parking areas and amenity spaces to ensure they are accessible via a range of modes. They should not simply be surrounded by car parking. To ensure that buildings front adjacent streets and internal drive aisles, more than one entrance and, therefore, more than one frontage may be required in some cases. Dual frontages and entrances improve site design flexibility and options for building location.

Offices should be laid-out as an integral part of the mixed-use commercial development and not planned as a separate business park location. For example, the area should link seamlessly with Main Street and the commercial uses along the Trans Canada highway so that workers can easily access retail facilities without the need to drive. There should also be convenient links to adjacent open spaces, including along Bush Creek, so that office workers can enjoy these spaces at lunchtime and during breaks.

Office buildings should play a role in defining street edges and pathways to ensure clear and pedestrian-friendly connections. Building set-backs should be kept to a minimum. Offices must not be designed as 'pavilion' style buildings surrounded by parking and incidental planting.

The aim is to create an active streetscape that provides retail and commercial services to local residents and tourists.



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1. There should be a continuous building frontage along urban streets to maintain a sense of enclosure
2. Outdoor seating areas can help to bring life and activity



### Strong street edge

Buildings of the commercial development will typically have retail at ground floor with residential or office above. These mixed-use buildings will play an important role in defining and enclosing the development. They will be built parallel to the right-of-way with zero set-backs. Building frontage should be continuous, with gaps between buildings kept to a minimum to maintain a sense of enclosure.

### Active frontages

Commercial mixed use buildings will help to animate and enliven public spaces at the heart of the village centre and or along the highway. In order to add interest, life and vitality to the street, building frontages will need to be active. This means:

- Frequent doors and windows with no blank walls. Large store windows allowing passers-by clear views inside and enabling communication between inside and outside.
- Articulating façade with awnings, colonnades or other projections that provide a more comfortable threshold in inclement weather and enable uses to overlap into the street.
- Providing 'spill out' space along the sidewalk, incorporating outdoor seating for cafés / restaurants or display space (eg. for fresh flowers).

### Building elements

- Elevations – the articulation of elevations should ensure they have relief, depth and shadow. This will give them visual interest from a range of viewing distances. It is important to emphasise vertical rhythm in particular and avoid blank walls and elevations that appear flat. Elevations of any significant size should be broken down through a combination of window openings, projections and recessions, and the use of different materials. The base, middle and top of the building should be clearly expressed through the use of materials and the design of architectural elements. There are opportunities to position interior activities that are more public – reception area, employee cafeteria, meeting spaces – on the ground floor and ensure they are visible from outside, rather than being positioned at the top of the building or buried in its



1. Buildings should have a clear base, middle and top
2. Awnings and canopies provide protection from the weather and can enhance the appearance of store fronts

interior. This will help to give life to surrounding streets.

- Entrances and access – store/ building entrances should be clearly expressed through appropriately designed canopies and signage and designed for universal accessibility. Colonnades and covered walkways are encouraged as a means of weather protection. Level changes from the sidewalk to entrances should be minimized to allow easy access for wheelchair users and people using baby strollers. Building entrances should face adjacent streets and have well-defined pedestrian access to sidewalks.
- Windows – there should be a high proportion of glazing along the façade of the building to ensure communication between the inside and outside of the building. Store front windows should always be transparent; faux glazing is not considered acceptable. Mirror and frosted glass should be avoided.
- Awnings and canopies – provide pedestrians with a comfortable environment protecting them from wind, rain and hot sun. Therefore, weather protection over sidewalks and entrances is to be provided as part of commercial retail frontage. It should be wide enough to shelter people from the weather; be mounted within the first floor level of a building; be designed to reflect the building's architectural style, giving consideration to integrating signage and lighting; and have supports that are slender, attractive and located to minimize impacts on pedestrian movement. Transparent canopies are to be used for the shady side of streets to offer more direct light to the sidewalk.
- Signage – signs are to be designed to complement the architecture of the building and not overwhelm the facade. The types of signs that are appropriate along Main Street are: banners; fascia signs; awning signs; canopy signs; window signs; and projecting signs. Signs that are externally lit should have fixtures that are appropriately scaled to the building and the signage. Over-illumination of signage is to be avoided.
- Roofs – flat or pitched roofs are considered appropriate. Flat roofs provide the opportunity for roof gardens associated with the upper level residential units. Where pitched roofs are used, a minimum 30 degree slope is recommended. Rooftop mechanical equipment should be integrated with the building design. However, regardless of the roof style the top of the building should be clearly articulated with an appropriate cornice or parapet. Rooftop plant should be an integral part of the design to limit its visual impact.



1. Corner buildings should positively address the street intersection
2. The design of commercial developments should promote walking and cycling



- Materials – buildings should have a simple high quality palette of materials, which when used in conjunction with each other should aim to break down the overall height and mass of the elevations. Timber, stone, brick and stucco are considered appropriate. Metal cladding systems are accepted if used in a warm and friendly manner. The range of materials used on any one building should be limited to create a sense of visual unity.

### Landscape design

Landscaping shall include a combination of trees and shrubs along the outside edge of the developments, and consist of predominantly native plant materials. Where plantings interface with retained native vegetation, new plantings shall replicate similar species composition to complement one another.

Street trees shall be planting at approximately 5 - 8 metres spacing along roadways. Trees are to be high headed with a minimum branching height (at maturity) that will not impede sightlines from street level into storefront doors or windows. Trees shall be installed in either tree grates, or within a landscaped boulevard of approximately 1.0 metre in width. Tree grates must be offset minimum 0.6 metres from the edge of sidewalk / curb interface, and tree wells are to be covered with a painted steel tree grate.

All sidewalks and trails along streets shall consist of unit pavers, concrete or asphalt. Unit paving materials and patterns must compliment the adjacent building facades and other development. Street furniture shall include benches and waste receptacles, with minimum two benches and one waste receptacle per block, per side of the street. Benches are to be located near crosswalk locations, or at building entrances and must suit the building façades.

Street lighting must consider prominent pedestrian activity and shed sufficient lighting on the sidewalk surface. Light standards should be pedestrian scale, and their style shall suit the adjacent building facades while being consistent throughout the development. Pole mounted banners are encouraged along the main street for establishing themes and celebrating annual events. Each tree grate shall be powered to allow for lighting of individual trees along the main street.

### Drive throughs



1. Store entrances should be clearly expressed
2. Drive throughs should be designed as an integral part of the primary building

Drive throughs should be designed as an integral part of the primary building, and should not appear simply as an 'add-on'. The visual impact of drive throughs should be limited through their careful siting, for example, they should be located away from site entrances and prominent corners.

Site circulation should be carefully considered and adequate space allowed for queuing traffic waiting to be served. Queuing traffic should not interfere with pedestrian movement or back out onto adjacent streets.

### Service areas

Service areas should be designed as an integral part of the overall site / building design and not regarded as an after-thought. Care should be taken to suitably screen service areas so that they are not visible from surrounding public areas or adjacent residential areas. Materials used for solid screen walls should be similar to those used on the building's exterior.

The visual impact of loading and service areas should be minimized through careful siting. There should be no open storage visible from the public realm.

### Parking

Parking for shoppers will be provided within urban streets in the form of parallel or angled parking stalls. Parking for business owners and residents of the upper level apartment units will be provided to the rear of the buildings.

On street parking areas require boulevard tree plantings, and trees should be offset from the curbs to prevent overhanging bumpers from meeting tree trunks. Off street parking areas are to be landscaped with a combination of shrubs and trees, with planting areas mulched with a stone mulch material on fabric.

Parking should include a combination of landscaping and pedestrian walkways to break down expansive areas of asphalt.

### Bicycle parking facilities

Bicycle racks should be provided to encourage people to cycle to Main Street.



1. Parallel or angle parking stalls shall be provided along urban streets
2. Bike racks shall be provided











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Hotel &  
Tourist Commercial

## Hotel & Tourist Commercial Development

In the mixed-use, tourist commercial areas it is possible that a range of uses may exist, including residential, commercial and tourist-orientated uses. Guidelines for residential development have already been outlined in Section 3. Guidelines for commercial mixed-use buildings (retail / residential and retail / office) have been outlined in Section 4. This section will, therefore, focus on tourist-orientated uses and the overall general character of the mixed-use, tourist commercial areas. Tourist-orientated uses may take the form of hotels, lodges and vacation homes, along with associated restaurants, cafes and convenience retail stores.

### Orientation and site layout

In order to appeal to tourists, areas should be pedestrian orientated and have a comfortable scale. A series of pedestrian pathways and trails through the area, connecting with the waterfront will be important. A variety of streets will add character and appeal. Some streets and spaces, especially those acting as focal points, should be lined with buildings to help give them life and vitality. It may more appropriate along other streets and spaces for buildings to be set back behind retained native planting. These streets may be more conducive to the creation of quieter, more relaxing areas for either residential or vacation homes.

### Scale and massing

The overall scale and massing of tourist-orientated buildings should be intimate. As with other buildings, the aim is to sensitively incorporate uses into the natural setting of the site. Retention of the natural character will be especially important in areas intended to appeal to tourists. Building of excessive mass or length could also block important views down to the waterfront.



1. There are opportunities for a mixture of uses in the tourist commercial areas
2. Buildings should be intimate in scale and be incorporated into the natural setting of the site



### Waterfront access

Maintaining public access along the waterfront will be important to ensure that everyone is able to enjoy this natural amenity. Private enclaves of development on the waterfront that prevent public access will not be permitted. Providing physical and visual connections down to the waterfront from further inland will be important; not only in terms of ensuring pedestrian access, but also in terms of the value of properties located further back from the waterfront.

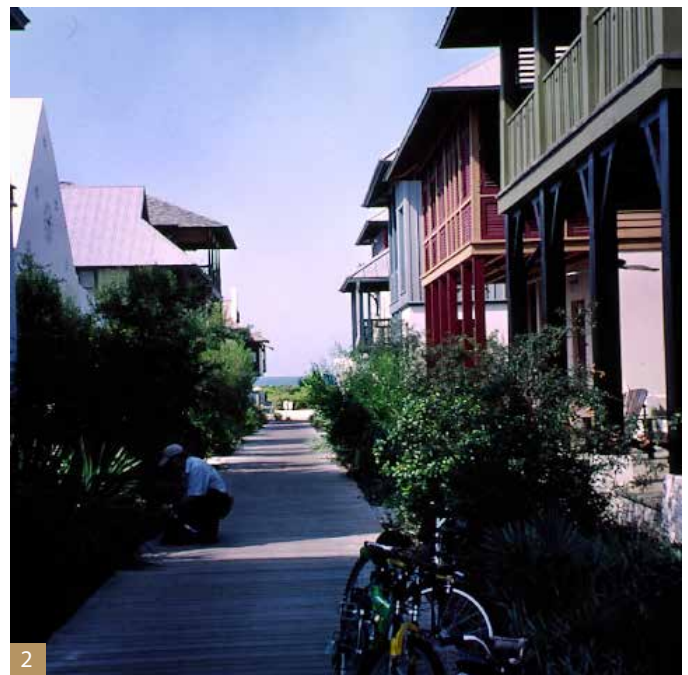
### Landscaping

Street trees shall be planting at approximately 8 metres spacing along roadways. Trees are to be high headed with a minimum branching height (at maturity) that will not impede sightlines from street level into store front doors or windows. Trees shall be installed in tree grates, offset minimum 0.6 metres from the edge of sidewalk / curb interface, and tree wells are to be covered with a painted steel tree grate.

All sidewalks and trails along streets shall consist of pavement such as concrete, asphalt or unit pavers. Unit paving materials and patterns must compliment the adjacent building facades and other development. Street furniture shall include benches and waste receptacles, with minimum two benches and one waste receptacle per block, per side of the street.

### Parking areas

On street parking areas must consider boulevard tree plantings, and trees should be offset from the curbs to prevent overhanging bumpers from meeting tree trunks. Off street parking areas are to be landscaped with a combination of shrub, and trees, and moved grass areas, with planting areas mulched with a wood fiber material.



1. Access to the water should be provided in the form of a series of pathways and trails
2. Physical and visual connections should be provided to the waterfront









# 6

## Parks and Open Space

## Parks and Open Space

### Residential

As residential developments are brought forward, 5% of the gross developable area should be dedicated to parks and open space.

Two playground structures should be created within the residential area, one on the east side and one on the west side of the proposed main street. Each playground should include equipment suited to both 0-5 and 5-12 years of age within the same equipment configuration. Surrounding park space should include seating facilities and formal planting schemes to support relaxation and enjoyment, while providing clear sightlines at eye level across the entire park. Parks shall have paved trails of approximately 2.0 metres width.

### Hotel & tourist commercial

Approximately 5% of this land area should be dedicated to parks and open space, including pocket parks and formal gathering spaces between buildings to encourage congregation of tourists and employees of commercial development. These parcels shall be formally designed to accommodate benches, waste receptacles, pedestrian scale lighting, and bicycle racks as needed. Planting schemes should include predominantly native plantings with mowed turf areas.



1



2

1. Playground structures should be provided within the residential areas
2. Park space should include seating facilities and formal planting



### Picnic area

The proposed picnic area shall include day use facilities such as vehicular parking, bicycle racks, picnic tables, waste receptacles, fire pits, a public water source, power supply to a portion of the sites, and a public washroom facility. This amenity shall provide year-round use for residents and tourists. Overnight use of the facility would be discouraged. The site shall be planted with native plantings within minimal mowed grass areas. Naturally occurring vegetation should be retained where possible.

### Environmentally sensitive area (east of Bush Creek)

Disturbance in this area will be limited to trail development. Any disturbed areas beyond the edge of the trail due to grading or staging areas must be limited where possible, and reclaimed using native plant materials.



1. The picnic area shall include facilities such as picnic tables, fire pits and washrooms, but will retain an informal feel
2. A new bridge link shall be provided across Bush Creek



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